



# Rhode Island Airport Corporation

December 7, 2023

## ADDENDUM NO. 4

Invitation for Bids No. 32231R

PVD Canopy Drainage Repairs and ADA Modifications

Rhode Island T. F. Green International Airport

1. **The following questions were submitted. RIAC's responses are in bold.**
  - a. At the Prebid walk through we noticed spalled and delaminated concrete at the departure level, should these be repaired as part of the ADA modifications? If yes what quantity should the bidders use?  
**This condition is outside the limit of the work and not required as part of the scope.**
  - b. What is the maximum number of Bays that can be closed at one time?  
**Only one bay may be closed at any given time. Refer to the General Phasing and Schedule notes on Drawing No. G0.01 for additional information.**
  - c. The note on C0.03 says "if the ramp requires reconstruction ...". How many new ramps should we figure on rebuilding in this bid?  
**The estimated quantity of ramps to be reconstructed, as shown on the bid form, is fourteen (14). This is our best estimate of the actual number of ramps to be reconstructed after the initial assessment is complete.**
  - d. Regarding the ramp detail on C0.04, what is the thickness of the ramp, is there a haunch at the curb line, what type of reinforcement is required, and how is the ramp doweled into the existing concrete?  
**It is the intent to replace the existing concrete section in kind, matching the adjacent concrete thickness. The replacement in kind also applies to any existing haunch behind the curb. Reinforcing wire has been added; refer to the revised Drawing No. C0.04 (attached). No doweling is proposed as part of the project.**

e. Can the bid date be extended?

**The bid date is not extended at this time.**

RIAC would like to remind all prospective bidders/offerors that additional Addendums may be issued by RIAC, at RIAC's sole discretion. As such, RIAC encourages prospective bidders/offerors to visit [www.flyri.com/riac/procurement](http://www.flyri.com/riac/procurement) on a frequent basis.

**####END OF ADDENDUM####**

RHODE ISLAND  
T. F. GREEN  
INTERNATIONAL  
AIRPORT  
WARWICK, RHODE ISLAND

STATE OF RHODE ISLAND  
RHODE ISLAND AIRPORT CORPORATION

Daniel McKee - Governor  
Jonathan N. Saragie - Chair, RIAC Board  
Iftikhar Ahmad - President and CEO  
Dawn Minister - Project Manager

PROJECT SUBMISSION PHASE:  
CONSTRUCTION DOCUMENTS

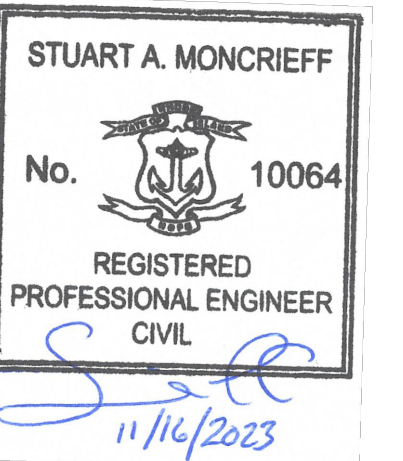
PRIMARY:

**Jacobs**

JACOBS ENGINEERING GROUP INC.  
120 ST JAMES AVE, 5th FLOOR  
BOSTON, MA 02116  
TEL: +1 617.242.9222

CONSULTANT:

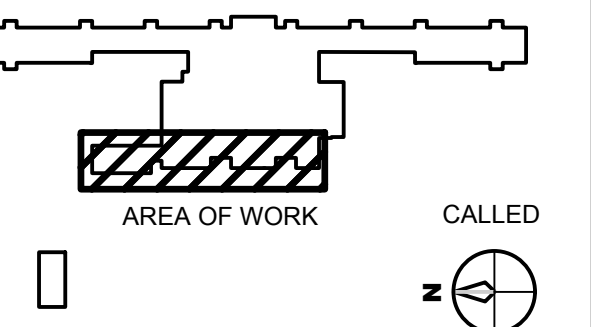
REGISTRATION STAMP:



REVISIONS:

REV. NO.	DATE	DESCRIPTION	BY
1	12/7/23	ADDENDUM #4	SAM

KEY PLAN:



PROJECT NUMBER AND TITLE:

RIAC CIP NO. PVD.340  
PVD CANOPY DRAINAGE REPAIRS  
AND ADA MODIFICATIONS

SHEET TITLE:

TYPICAL DETAILS  
(1 OF 2)

DISCIPLINE:

DESIGN BY:	DRAWN BY:	CHECKED BY:	APPROVED BY:
WH	WH	SM	SM

SCALE:

NTS

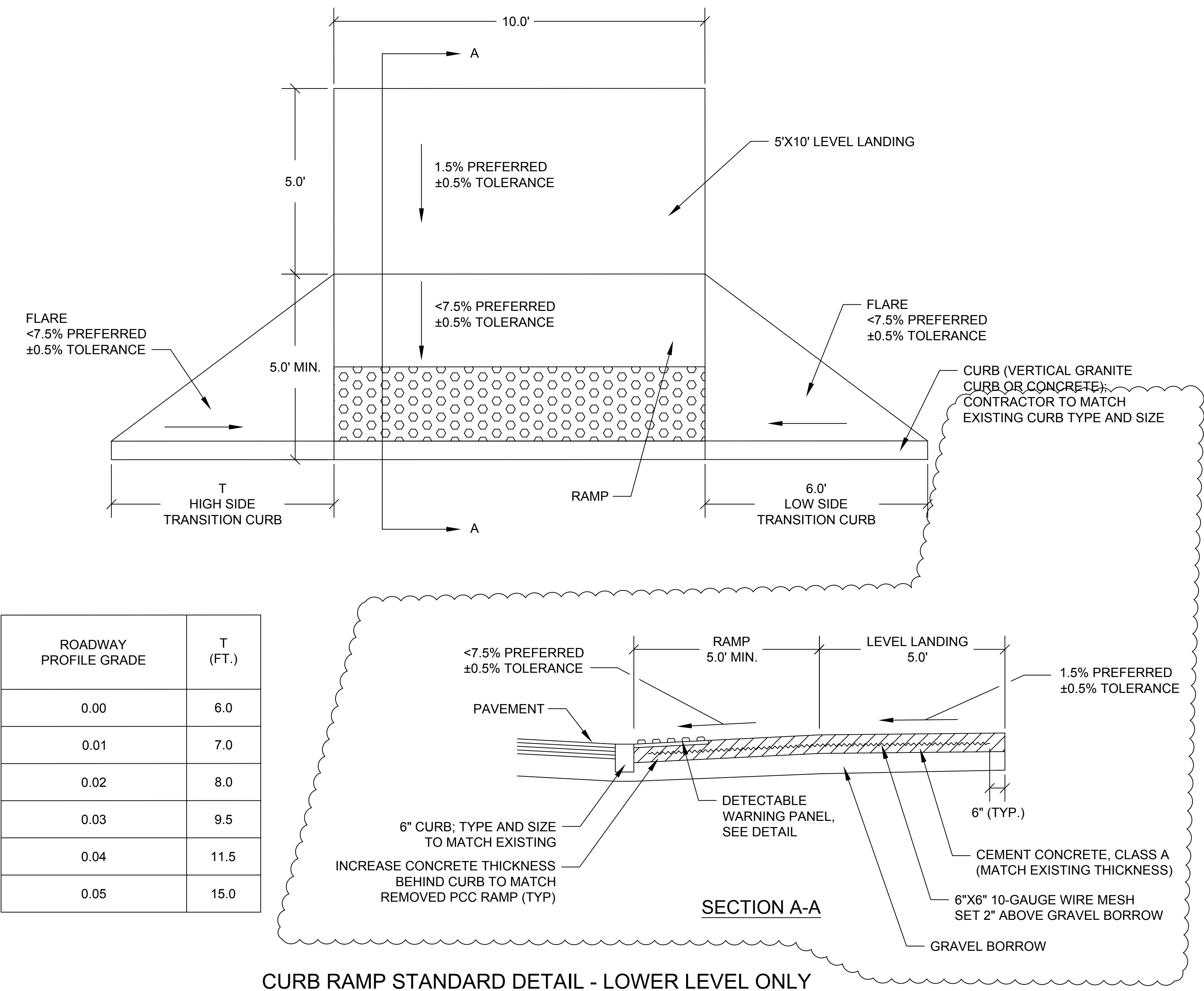
DATE:

11/16/23

SHEET NUMBER:

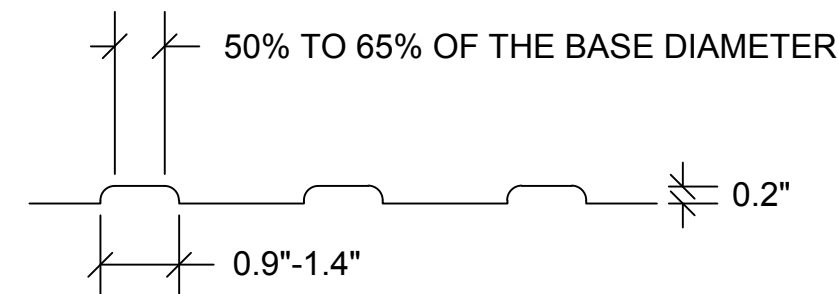
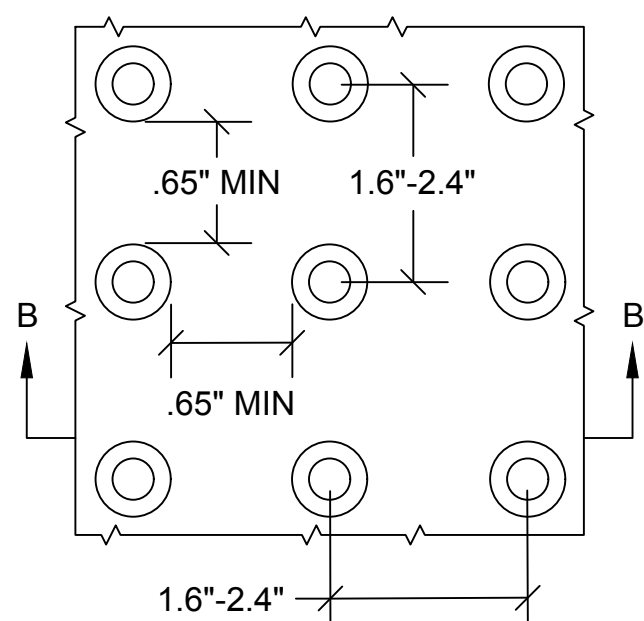
SHEET 19 OF 20

C0.04



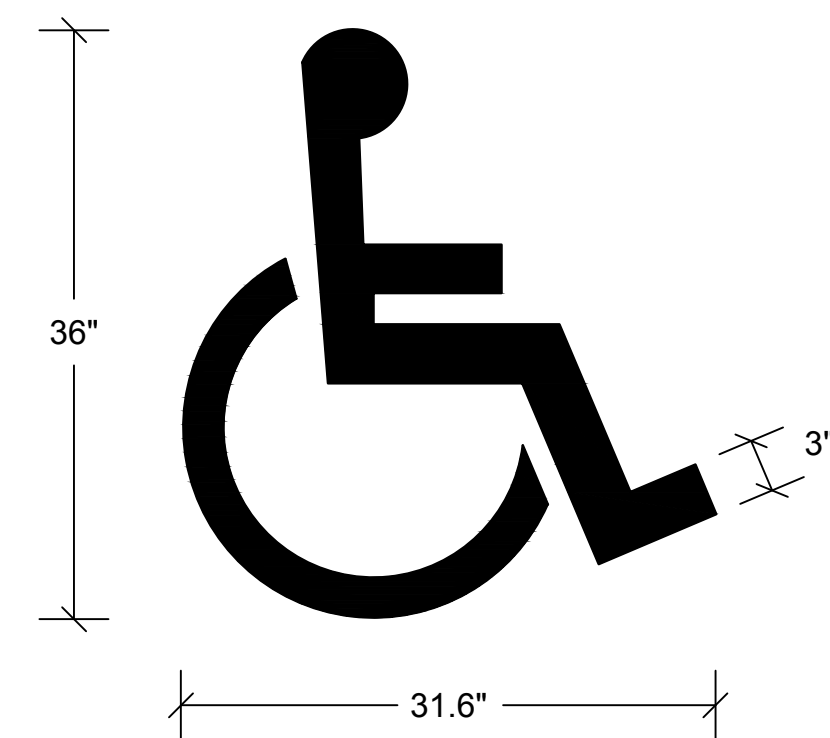
CURB RAMP NOTES:

- SIDEWALK CROSS SLOPES WILL BE 1.5% (±0.5%), 1.5% PREFERRED. IN NO INSTANCE SHALL THE SIDEWALK CROSS SLOPE EXCEED 2.0%.
- FIXED OBJECTS (E.G. BOLLARDS, SIGN POSTS) SHALL NOT BE LOCATED IN LEVEL LANDINGS, LEVEL ENTRANCES, AND RAMPS.
- THE CURB RAMP SLOPES AND SIDE SLOPES (TRANSITIONS) SHALL BE 7.5% (±0.5%), 7.5% PREFERRED. THESE SLOPES MAY BE FLATTER WHEN WARRANTED BY SURROUNDING CONDITIONS.
- IN AREAS WHERE THE RAMP IS LESS THAN 15 FEET AND THE ROADWAY PROFILE IS LESS THAN 4%, THE MAXIMUM SLOPE OF THE RAMP SHALL NOT EXCEED 8.3%.
- IN NO CASE WHERE A CROSSWALK IS PROVIDED SHALL ANY PART OF THE CURB RAMP, EXCLUDING CURB TRANSITIONS, BE LOCATED OUTSIDE THE CROSSWALK.
- THE ENTRANCE OF A CURB RAMP SHALL BE FLUSH WITH THE ROADWAY.
- TESTING SURFACE: WHEN TESTING WITH A STRAIGHT EDGE PLACED PARALLEL TO THE LINE OF THE SLOPE, THERE SHALL BE NO DEVIATION FROM A TRUE SURFACE IN EXCESS OF 1/4".
- DETECTABLE WARNING PANELS SHALL BE SET BACK FROM THE EDGE OF ROADWAY A DISTANCE OF 6".
- LEVEL LANDING AND LEVEL ENTRANCE SLOPES SHALL BE 1.5% (±0.5%), 1.5% PREFERRED. IN NO CASE SHALL THE SLOPE EXCEED 2.0% IN ANY DIRECTION.



SECTION B-B

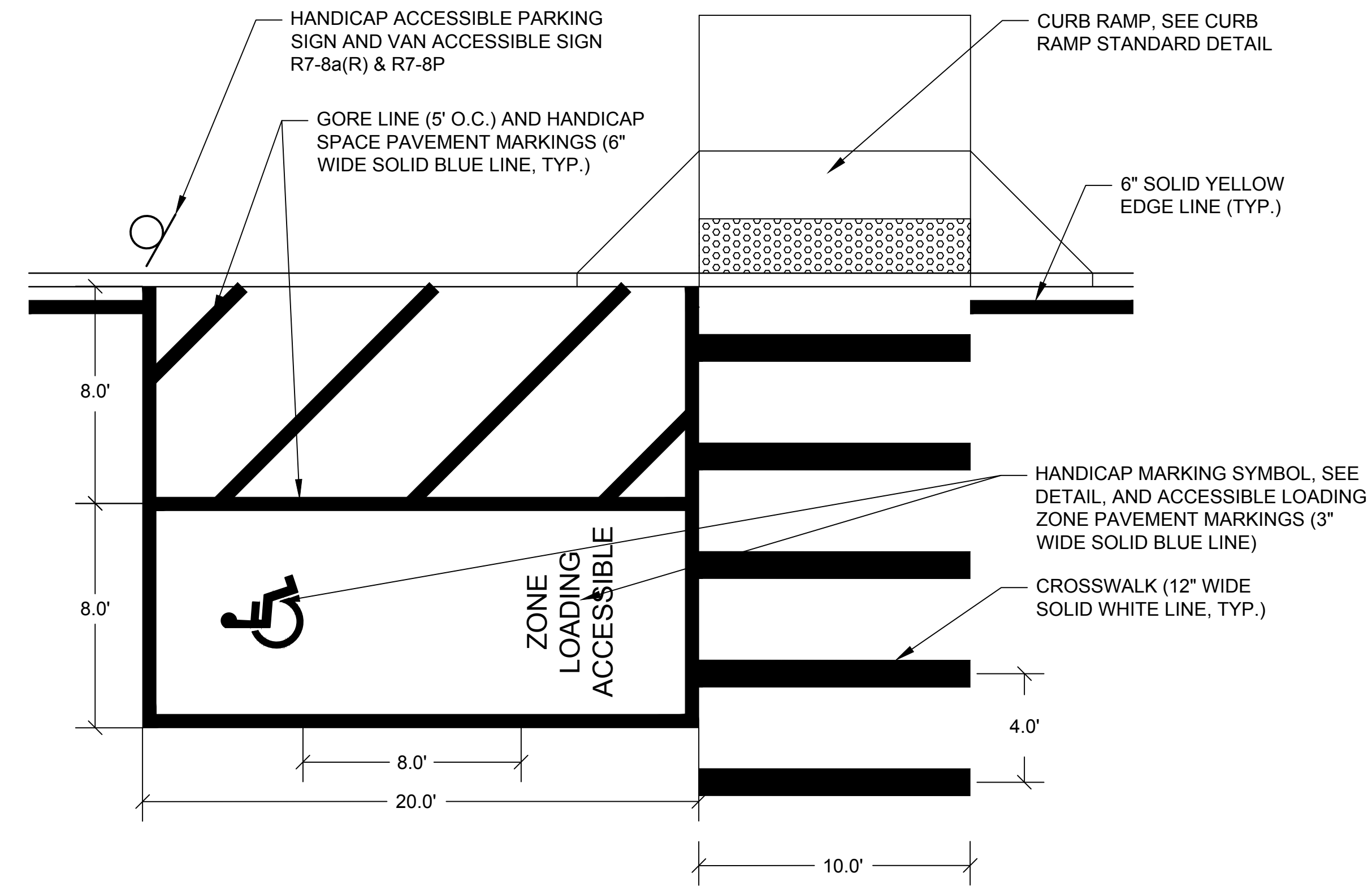
DETECTABLE WARNING PANEL DETAIL



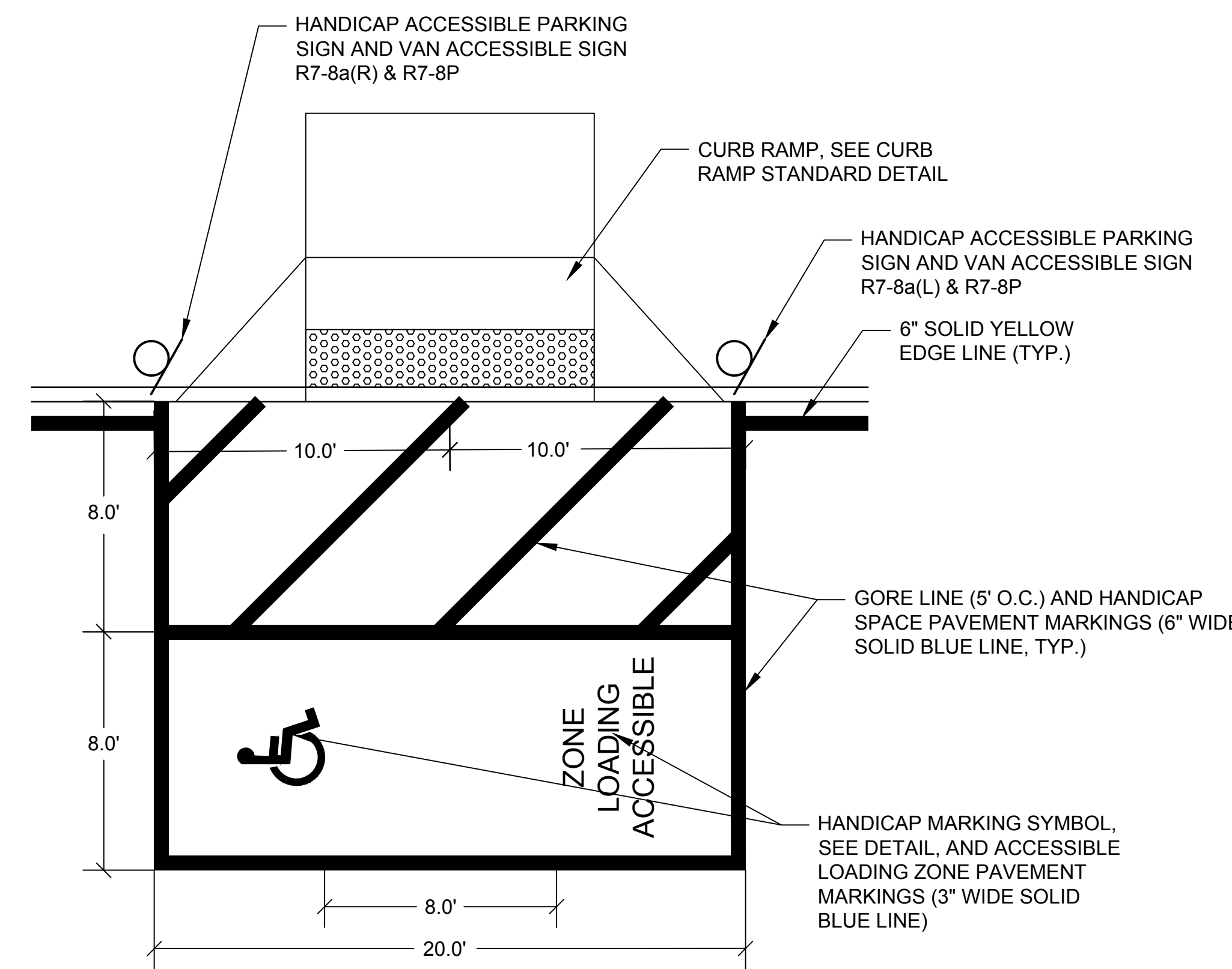
HANDICAP MARKING SYMBOL NOTES:

- HANDICAP MARKING SYMBOL TO COMPLY WITH 2010 ADA STANDARDS FOR ACCESSIBLE DESIGN SECTION 703.7.2.1 INTERNATIONAL SYMBOL OF ACCESSIBILITY.
- PAVEMENT MARKING COLOR SHALL BE FS 15090 IN FEDERAL STANDARD 595C.

HANDICAP MARKING SYMBOL



LOADING ZONE ADJACENT TO CROSSWALK  
STANDARD DETAIL



LOADING ZONE WIHTOUT CROSSWALK  
STANDARD DETAIL